

Land Transport Rule: Traffic Control Devices 2004 - Directional Cycle Traffic Signals Trial

Pursuant to subclause 3.4(1) of the Land Transport Rule: Traffic Control Devices 2004 (“Rule”) and a delegation from the NZ Transport Agency, I, Glenn Bunting, Network Manager, authorise the installation and maintenance of directional cycle traffic signal displays, incorporating a cycle symbol and an arrow, to control the movement of cycle traffic in different directions at different times:

- a. for the purpose described in Schedule 1;
- b. in the form and layout set out in Schedule 2;
- c. at the locations stated in Schedule 3;
- d. for the period specified in Schedule 4; and
- e. subject to the evaluation outlined in Schedule 5.

The directional cycle traffic signal displays may be installed for the purpose of evaluating their use and the trial will be called the “directional cycle traffic signals trial”.

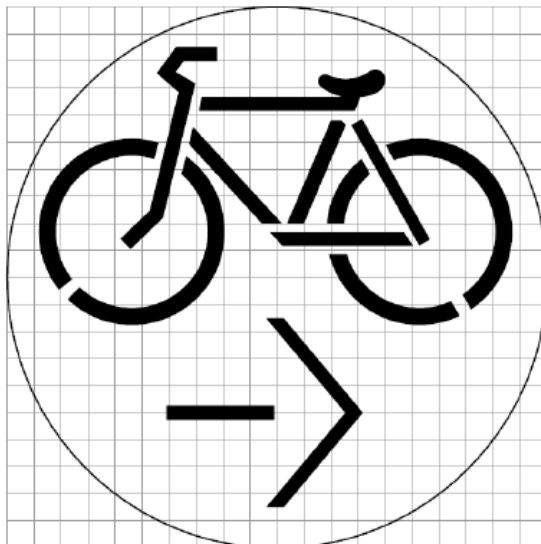
Schedule 1—Purpose of trial

The purpose of the trial is to:

- a. Enable the installation of directional cycle traffic signal displays that incorporate an arrow in the same signal aspect with the cycle symbol, as an alternative to the requirements for traffic signal displays in 6.3(1)(a), 6.4(1) and Schedule 3 of the Rule;
- b. evaluate the safety and effectiveness of traffic signal displays that allow cyclists on the same approach to an intersection to move in different directions at different times; and
- c. assess cyclists’ and other road users’ understanding of and compliance with traffic signals that control directional movement of cyclists at intersections with cycle traffic signals.

Schedule 2—Form and layout of directional cycle traffic signals

- a. Each red, yellow and green directional cycle traffic signal display must comprise a cycle symbol above an arrow to indicate the controlled movement;
- b. directional cycle traffic signal displays must be at least 200mm diameter;
- c. the sequence of directional cycle traffic signals must comply with 6.4(3) of the Rule;
- d. the operation of the signals must comply with clauses 6.4(4)-6.4(8) of the Rule; and
- e. directional cycle traffic signals may only be installed at an intersection in conjunction with standard displays of traffic signals for vehicles.



Example: Directional cycle traffic signal display.

Note: The example directional cycle traffic signal display is conceptual only and is not intended to be the design standard used for the trial.

Schedule 3—Location

The locations approved for this trial are:

Road Controlling Authority	Location
Auckland Transport	Beach Road and Te Taou Crescent intersection
Auckland Transport	Nelson Street and Victoria Street intersection
Christchurch City Council	Antigua Street and St Asaph Street intersection
Christchurch City Council	High Street, Madras Street and St Asaph Street intersection

Schedule 4—Period of trial

The trial may begin after 1 July 2017 and, unless terminated earlier, must end by 30 June 2019.

Schedule 5—Evaluation

An evaluation and analysis must be undertaken and reports prepared as described in the trial application dated February 2017, received from the Christchurch City Council. The evaluation must include:

- a. Measurement of cyclists' compliance with the directional cycle signals;
- b. measurement of other road users' compliance with the traffic signals at intersections where directional cycle signals are installed;
- c. assessment of road users' recognition and understanding of the displays used for directional cycle signals;
- d. analysis of conflicts and crashes;
- e. an outline of costs, efficiency improvements and other benefits;
- f. a summary of feedback received from cyclists, and the general public during the trial;
- g. a description of any operational issues which arose during the trial and how they were resolved; and
- h. recommendations for improvements or changes that should be made to the operation, form or layout of directional cycle traffic signals.

An interim evaluation report must be sent to me by 28 February 2018 and a final report by 31 August 2019.

Signed at Wellington this 22nd day of May 2017.

GLENN BUNTING, Network Manager, NZ Transport Agency.