

Limit Line Pavement Markers Trial

Pursuant to subclause 3.4(1) of Land Transport Rule: Traffic Control Devices 2004 (“the Rule”) and a delegation from the NZ Transport Agency, I, Glenn Bunting, Network Manager, authorise the installation and maintenance of red, illuminated pavement markers at traffic signal limit lines:

- a. for the purpose described in Schedule 1;
- b. in the form and layout set out in Schedule 2;
- c. at the location stated in Schedule 3;
- d. for the period specified in Schedule 4; and
- e. subject to the conditions and an evaluation outlined in Schedule 5.

The red, illuminated pavement markers may be installed for the purpose of evaluating their use and the trial will be called the “limit line pavement markers trial”.

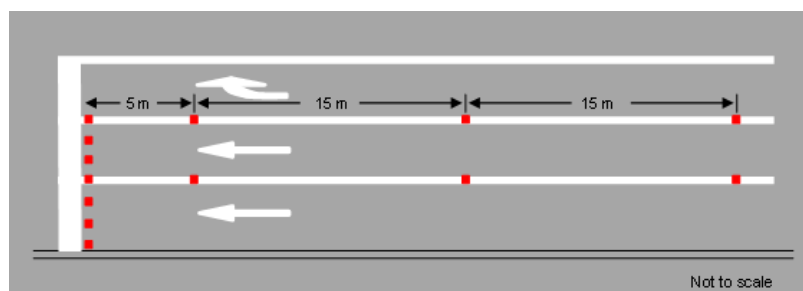
Schedule 1—Purpose of trial

The purpose of the trial is to:

- a. evaluate the effectiveness and safety of red, illuminated pavement markers installed on the limit line at traffic signals;
- b. enable the placement of red pavement markers in an alternative position to the left edge of the roadway, as specified in subclause 5.4(6)(b) of the Rule; and
- c. assess drivers’ understanding of red, illuminated limit line pavement markers and compliance with red traffic signals which are augmented by red, illuminated limit line pavement markers.

Schedule 2—Form and layout of red, illuminated limit line pavement markers

- a. Red, illuminated pavement markers used in this trial must be approximately the same shape and size as standard reflectorised pavement markers;
- b. red, illuminated pavement markers may be installed on the limit line or immediately before the limit line, on the straight-through lanes at traffic signals, facing traffic approaching the traffic signals;
- c. a minimum of four pavement markers must be installed, evenly spaced, across the full width of each lane; and
- d. three pavement markers may be installed on each lane line for the straight-through lanes as follows:
 - i. one pavement marker 5 metres in advance of the limit line;
 - ii. one pavement marker 20 metres in advance of the limit line;
 - iii. one pavement marker 35 metres in advance of the limit line.



Layout diagram for red, illuminated limit line pavement markers

Schedule 3—Location

The location approved for this trial is the intersection of Remuera Road, Clonbern Road and Victoria Avenue, Auckland.

Schedule 4—Period of trial

The trial may begin after the publication of this notice and, unless terminated earlier, must end by **30 September 2016**.

Schedule 5—Conditions and evaluation

- a. The illumination of the pavement markers must be linked to the traffic signal controller so that:
 - i. the pavement markers are illuminated red when the lanes in which they are installed are controlled by a red traffic signal; and
 - ii. the pavement markers are not illuminated when the lanes in which they are installed are controlled by a green or yellow traffic signal.
- b. A missing, damaged or malfunctioning pavement marker must be repaired or replaced as soon as possible during the trial.
- c. An evaluation must be undertaken as outlined in Auckland Transport's trial application of September 2014 and include:
 - i. a measurement of any change in drivers' compliance with the traffic signals;
 - ii. an assessment of drivers' understanding of the red, illuminated limit line pavement markers;
 - iii. a description of the method of installing the markers, including how they were linked to the traffic signal controller;
 - iv. a description of any operational problems that arose during the trial and how they were resolved;
 - v. recommendations for improvements in the installation and operation for future use of illuminated limit line pavement markers; and
 - vi. an assessment of costs and benefits, including any crash reduction attributable to illuminated limit line pavement markers.

An interim evaluation report must be sent to me by 29 February 2016 and a final report by 30 November 2016.

Signed at Wellington this 24th day of July 2015.

GLENN BUNTING, Network Manager.